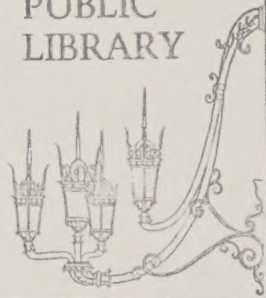
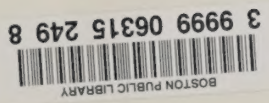


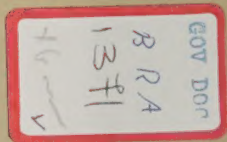
BOSTON
PUBLIC
LIBRARY





copley place

BOSTON PUBLIC LIBRARY



INDIAN REVIEW

INDIAN REVIEW

BIND Along This
← edge

SUBCOMMITTEE

SUBCOMMITTEE

Copley Place
(Back Bay)

C 50 C
DR

report of the

introduction


findings

recommendations

appendices

HEMAN REVIEW
HEMAN REVIEW
HEMAN REVIEW
SUBMITTIES
SUBMITTIES

chapter review committee September 24, 1940



Digitized by the Internet Archive
in 2023 with funding from
Boston Public Library

https://archive.org/details/reportofdesignre00copl_0

report of the

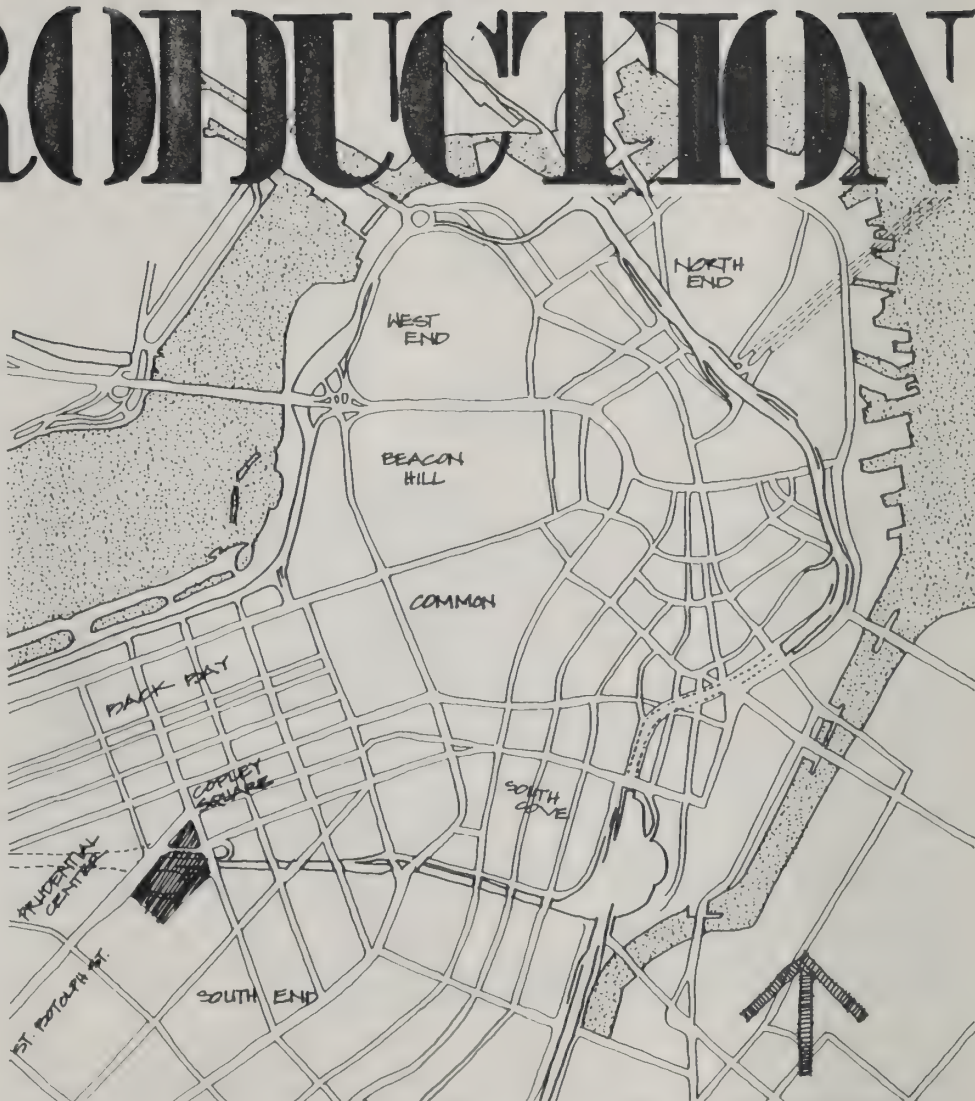
DESIGN REVIEW
DESIGN REVIEW
DESIGN REVIEW
SUBCOMMITTEE
SUBCOMMITTEE

.....copley place citizens' review committee september 24, 1980.....

INTRODUCTION

This is the report of the Design Review Subcommittee to the Copley Place Citizens Review Committee. The DRS was established in the terms of the lease between the Massachusetts Turnpike Authority and UIDC, as the advisory body to the Turnpike on the design of Copley Place. The DRS's charge is to interpret the CRC Guidelines as they apply to Copley Place design, to apply its professional judgement to design related issues called out in the lease, and to comment on other design-related concerns.

The findings discussed in the second section of this report, and the recommendations made in the third part, refer to "Final Design Plans" presented by Urban Investment & Development Co. to the Massachusetts Turnpike Authority and Design Review Subcommittee on September 11, 1980. These plans are partial revisions to plans presented to the Citizens Review Committee July 17, 1980. The "Final Design Plans" are not construction documents. Changes are likely to be made in the design as shown. Some parts of the project will change more than others.



Recognizing this, the DRS comments in the Findings section on the suitability of the design in light of the guidelines, and makes its recommendations based on the varying levels of detail presented in the "Final Design Plans".

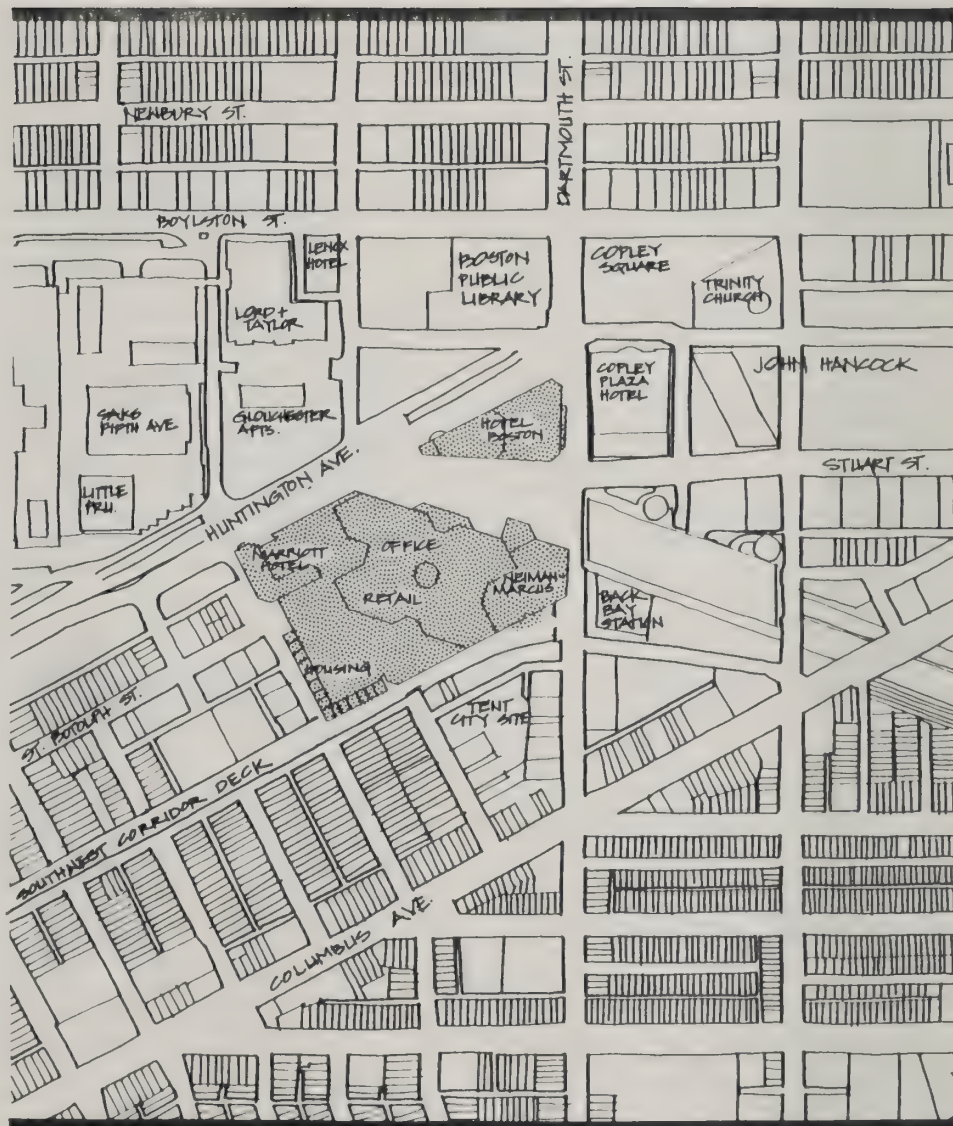
The subcommittee's overriding concern has been with the street and neighborhood design impacts of the project. Although parts of the "Final Design Plans" show significant improvement over earlier treatments, two major problems remain.

1. Design Treatments:

a. scale transition between the mass of Copley Place and the varying urban textures on project edges is unsuccessful in many areas. Massing, facade treatments, articulation, and materials are all part of this.

b. pedestrian ways around and through the project, particularly along Huntington Ave. do not provide an acceptably humane pedestrian environment, and do not sufficiently continue the pedestrian-oriented character of the adjacent Back Bay, South End and St. Botolph neighborhoods.

c. The overriding direction of the design is inward, away from the city and the neighborhoods. Activity is consistently directed away from the street, thus rejecting the project's



opportunity to re-link the residential and commercial neighborhoods around it.

2. Design Decision Timing:

Many of the design decisions which could reduce the impact of current problems have not yet been made. Materials, some massing, pedestrian wind problem treatment, and landscaping are still being refined.

NEXT STEPS

1. After presentation of this report to the CRC and MTA, the MTA will transmit its Design Review comments to the developer by October 11. The DRS recommendations and comments should be coordinated with other ongoing design review processes.

2. Continuing Design Review will be provided by the City of Boston through the Boston Redevelopment Authority, and by the Massachusetts Historical Commission through the 106 Review Process.

3. City and MTA must resolve the pedestrian access question near the ramp exit at Stuart St.

4. The developer and relevant public agencies should finalize coordination of landscaping, Southwest Corridor deck financing, Back Bay Station underpass financing, pedestrian street crossing and other public actions directly relating to the implementation of the design as presented.

This report was prepared by Tunney Lee, Deborah Poodry and Patricia McDaniel, under contract to the Massachusetts Turnpike Authority.

This report reviews the mandate of the DRS, examines the design in terms of CRC and lease guidelines, sets forth three categories of recommendations to the MTA, and reviews the DRS process to date.

PROCESS

Since the last meeting of the full CRC, the Design Review Subcommittee has met several times with the architect and developer to discuss and respond to details of the design. Summaries of the meetings are included in this report, and full stenographic records of the meetings are available through UIDC. (see appendix)

SCHEDULE

July 17	General CRC Meeting: Project update, design presentation; kick-off for Subcommittee
July 24	Design Review Subcommittee Meeting: Massing
July 31	Design Review Subcommittee Meeting: Massing
August 7	Design Review Subcommittee Meeting: Materials, pedestrian circulation, edges, interior way
August 21	Design Review Subcommittee Meeting: Pedestrian linkages, Housing
August 28	Design Review Subcommittee Meeting: Response to concerns presented by Subcommittee
Sept. 11	Design Review Subcommittee Meeting: Presentation of revised plans.
Sept. 24	General CRC Meeting: Presentation of Revised Plans and DRS Report

CHARGE TO THE SUBCOMMITTEE

The subcommittee is charged by the full CRC lease to review the project's compliance with the design guidelines as set forth in the lease, the amended lease and the CRC's Final Recommendations, September 22, 1977.

"The Citizen's Review Committee may advise the Landlord and the Tenant on such matters relating to the development as the Landlord may prescribe, including but not limited to the following:

(a) the relationship of the proposed deck over the Southwest Corridor transit line to be constructed by the Massachusetts Bay Transportation Authority from Dartmouth Street to Yarmouth Street, to the development;

(b) construction of at grade and below grade pedestrian connections between the development and the Back Bay Station;...

(c) construction of pedestrian connections between the development and the Prudential Center Area; and

(d) the development of advisory programs to assist the Tenant and the City of Boston in mitigating any potential adverse parking impacts the development may have on surrounding neighborhoods.

Said Design Review Subcommittee will be responsible for advising Landlord with respect to the Guidelines contained in the Final Recommendations for Copley Square submitted by the Copley Square Citizens' Review Committee on September 22, 1977, specifically with respect to, but not limited to, the following:

(a) providing for active retail uses especially along Dartmouth and Stuart Streets, and active uses and other visual interests along all portions of the development abutting public streets;

(b) providing for a compatible integration of the development which complements and reinforces the scale and texture of surrounding areas, particularly with respect to the treatment of those portions of the development which are visible to the surrounding residential areas;

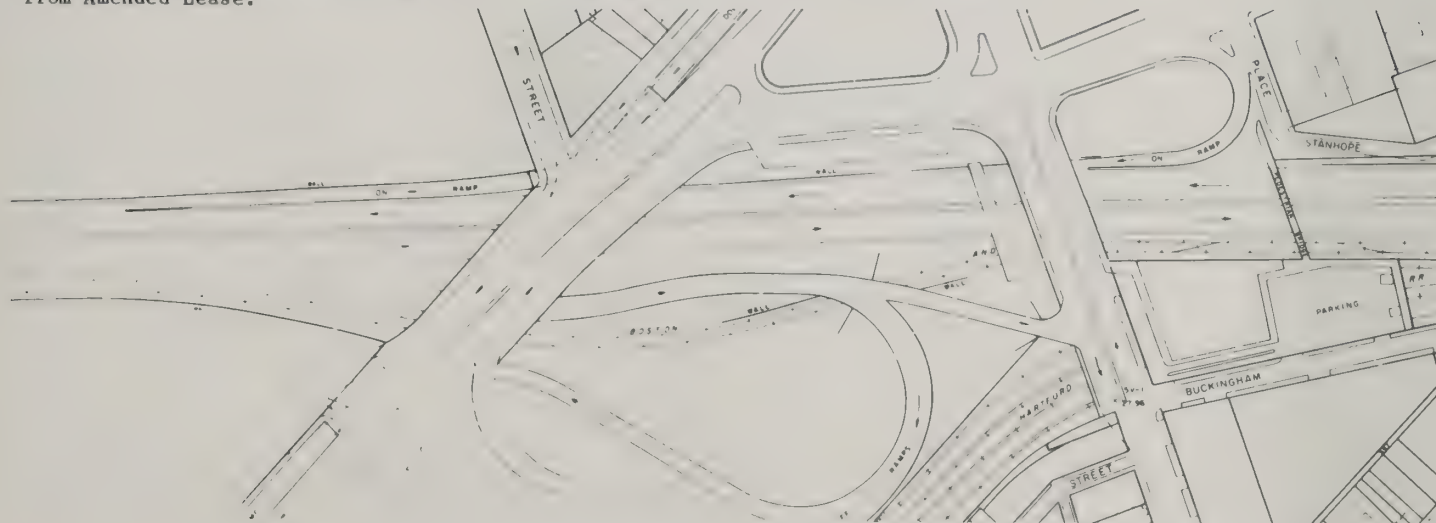
(c) providing for pedestrian movement and linkage from the Boston Public Library to Columbus Avenue;

(d) providing an adequate number of pedestrian rights-of-way around and through the development, which shall be well lit, clearly marked, accessible to the handicapped, and which protect the pedestrian from excessive wind turbulence, noise and fumes, including the provision of a landscaped walkway from Harcourt Street at the end of St. Botolph Street to Huntington Avenue; and

(e) providing for the softening of facades of all buildings through the use of appropriate materials, irregular setbacks, landscaping, and the like.

In matters such as massing, access, live edges, underpasses, landscaping, vehicle access points and the like, the Tenant will be guided in its design and the Landlord in its design review by the concepts which have arisen through the Citizens' Review process as reflected in the published recommendations resulting from such process, but will not be bound to do so whenever, in the judgment of the Authority, it would be inconsistent or in conflict with its primary duty to provide for safe, continuous and uninterrupted operation of the Boston Extension of the Massachusetts Turnpike Authority."

from Amended Lease.



..PROJECT DESCRIPTION.....

The Copley Place project is being developed by the Urban Investment and Development Company on 9.5 acres adjacent to and over the Massachusetts Turnpike exit ramps in Boston's Back Bay.

The project, to be constructed over 3 1/2 year period, will include 3,434,060 gross feet of hotel, retail, office, parking, and housing uses, and has a projected cost of \$318 million. The Architects Collaborative is responsible for the design of the project; Victor Vitols Associates are working on the design of the housing component.

	From CRC Final Recommendations 9/22/77	Copley EIS (1) 6/78	Copley Place CRC Meeting Handout 5/31/79 Copley EIS (2) 2/80	Copley Place Project Information 7/80
Hotel 1	800 rooms	868	712	781
Hotel 2	712,000	- -	960	1,008
Retail	590,000	672,600	517,000	520,630
(dept. Store)	(225,000)	(320,000)	(112,630)	
Office	600,000	612,000	728,110	771,400
Parking	375,000	525,000	517,120	505,460
Hotel 1	1,000 spaces	1,400 sp.	333 sp.	275 sp.
Other			1,100 sp.	1,157 sp.
			1,433 sp.	1,432 sp.
Housing		100-150 units	100 units	100 units
TOTAL	2,277,000	3,244,800	3,211,900	3,434,060

The program is outlined below

PROGRAM SUMMARY, 9/11/80				
	Total Building Area S.F.	Rentable Area S.F.	Service Level, Mall Circulation, Service Corridors, Mechanical	
Hotel #1 Western International			TOTAL RETAIL	520,630 370,510
781 rooms	747,260	- -		
Parking - 275 spaces	100,540		Office	771,400 699,940
SUB TOTAL	847,800	- -		
Hotel #2 Marriott			Parking - (Hotel #2, plus parking under Retail/Office)	
1,008 rooms	800,310		1,157 Spaces	404,920 - -
Retail			Housing	
Cinemas	16,000	16,000	100 Units	85,000 - -
Specialty Department Store	96,090	96,090		
Mall Shops	250,420	250,420	TOTAL AREA	3,434,060 1,070,450
Community Retail	8,000	8,000		

- 1 HOTEL BOSTON
- 2 MARRIOTT HOTEL
- 3 OFFICE TOWERS [RETAIL + PARKING BELOW]
- 4 NEIGHBORHOOD RETAIL
- 5 NEIMAN-MARCUS
- 6 PLAZA
- 7 MALL ENTRY
- 8 HOUSING
- 9 TURNPIKE EXHAUST VENT
- 10 PEDESTRIAN BRIDGE



PLAN

OTHER DESIGN REVIEW ACTIVITIES



In addition to the CRC process, the following groups are also reviewing Copley Place design.

The Boston Redevelopment Authority (BRA) is currently reviewing the plans and outline specifications associated with the early stages of design development for the major components of the project. The BRA as the City's Planning Department will require submission of plans and other project descriptions at three further stages of architectural design: 1) final design development, 2) preliminary working drawings, and 3) final contract documents. Submissions will be reviewed in timely fashion and approved or disapproved in writing. In undertaking these reviews the BRA will remain abreast of the DRS and MTA's positions and will carefully consider their concerns before giving final approval for the

project to proceed. When all submissions have been revised and approved, construction of the project will be monitored to insure conformity with the approved plans and specifications.

The City of Boston has the authority to release UDAG funds, at its discretion, if the UDAG is approved, and also can provide the project with a property tax agreement.

106 Historic Properties Review Process

The Copley Place site is adjacent to several historic districts and several landmarks on the National Register of Historic Properties. Therefore, a Section 106 Review by the Massachusetts Historic Preservation Officer and the U.S. Advisory Council on Historic

Preservation must be accomplished prior to the start of construction. The 106 process was initiated in the summer of 1979 by presenting the basic design of the Project to the Massachusetts Historical Commission and the Boston Redevelopment Authority. The Commission concluded after the presentation that although the design appeared to be sensitive to the surrounding properties, not enough details were as yet available to make a final determination. Details required include information on materials to be used on the exterior of the buildings, design details, facade and fenestration details, and colors of the materials to be used on the exterior of the buildings.

FINDINGS

MASSING SCALE FORM & ORIENTATION

FINDINGS

The project, as presently designed, does not adequately resolve the transition in scale from the high towers near the Prudential and Hancock complexes to the small-scale residential areas of the South End and St. Botolph Sts.

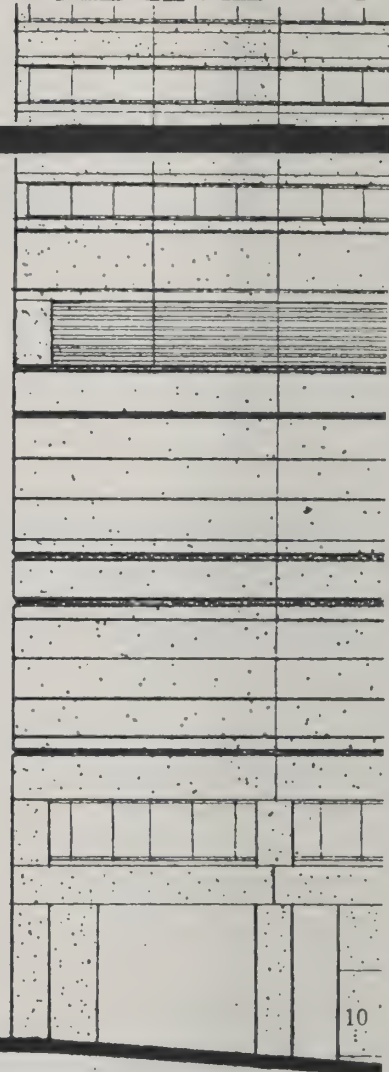
The location of uses on the site is appropriate, and the numerical height and setback guidelines have been met, but the critical element of architectural treatment is not satisfactorily refined.

SCALE

A. Hotel Boston Facade onto Copley Square: This facade is essentially two layers - glass near the ground and concrete above. This makes it appear larger than and out of character with the adjacent Copley Plaza and Boston Public Library facades - which read as walls with openings.

RELEVANT GUIDELINES

1. To fill a major gap in the existing neighborhood and urban texture; to complement and reinforce the scale and texture of surrounding areas and adjacent block patterns.
 2. To provide active frontage along city streets now "one-sided"; to restore Copley Square as an enclosed space; to provide a transition in scale from the low rise residential areas south and east of the site.
 3. To preserve the scale and texture of the existing areas from being destroyed by new construction that is too tall, bulky or inappropriately scaled.
2. Triangular site -- cornice line of building base to be no lower than the cornice line of Public Library and no higher than the Copley Plaza Hotel. Triangular site tower -- no higher than 390' above the adjacent street.



FINDINGS

The hotel's glass sheds at street level along Dartmouth are a desirable addition to street activity.

B. Southwest Corridor Deck Facade:

This facade looms jarringly above the small scale brick townhouses of the South End. The Copley Place housing and neighborhood retail space do not extend far enough along the facade to relieve it.

The height of the office block, which is appropriate on other parts of the site, is too high adjacent to the South End.

Expression of activities behind the facade has significantly improved over previous plans.

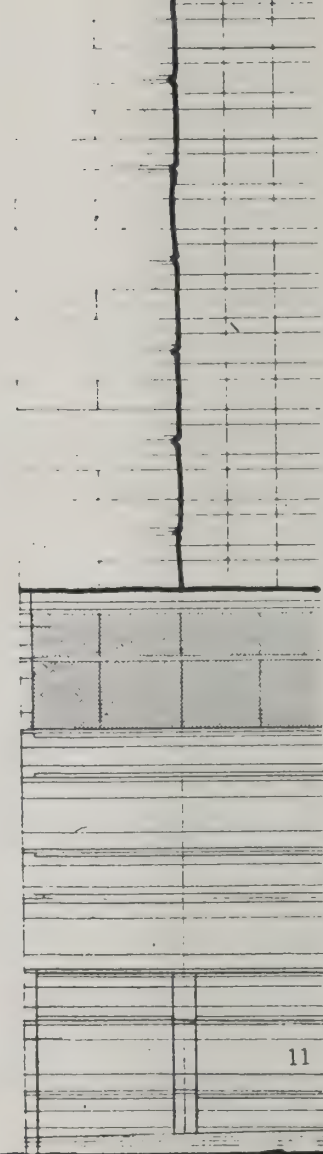
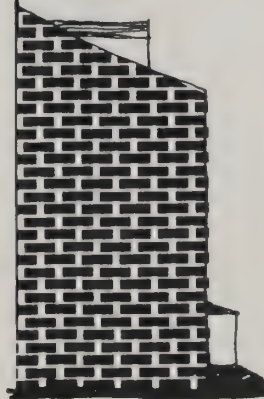
The scale of elements near the pedestrian route along the cover is too large, particularly in front of the Turnpike ramp, where an almost unbroken wall of approximately 60' x 210' faces onto the deck and the Tent City housing site.

RELEVANT GUIDELINES

On the triangular site, any tower or other element higher than the cornice lines of the Copley Plaza Hotel must be set significantly back from that corner of the triangle site which faces Copley Square.

No building on the site shall be more than 390 feet higher than the adjacent street.

MBTA track cover edge between Yarmouth and Dartmouth -- height of buildings along this edge should recognize: existing South End scale; any development on the Tent City site; the transitional nature of this area from the South End to Back Bay.



FINDINGS

RELEVANT GUIDELINES

C. Marriott Hotel, Harcourt St. Facade:

Treatment of massing and facade on this edge is critical to maintaining the scale and character of St. Botolph St. and the usefulness of a major pedestrian route along Harcourt St.

The treatment of scale, materials and visible activity along this facade, to the top of the podium, does not make a sufficient transition between the bulk of the hotel tower and the adjoining pedestrian oriented residential areas.

D. Housing:

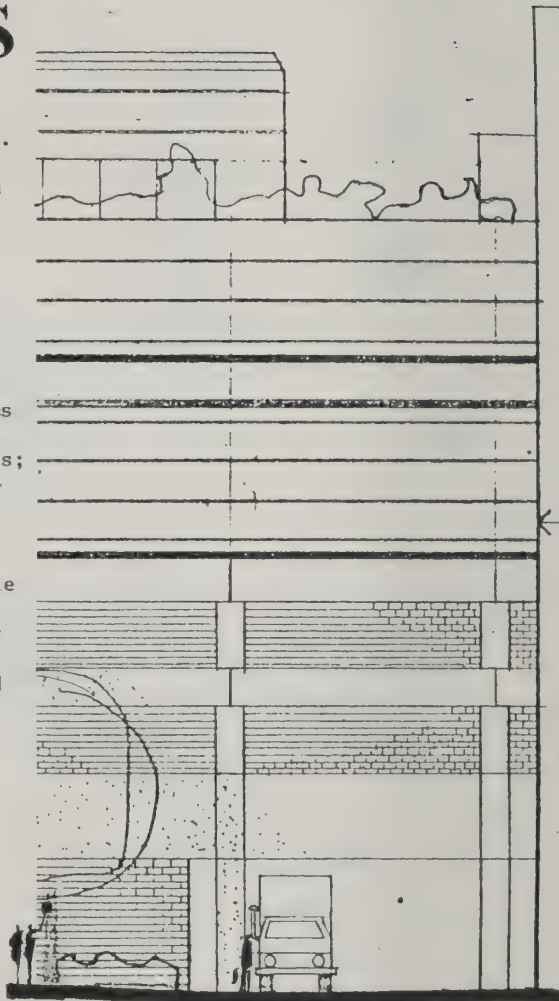
Although it is within the height limit set by the guidelines, the height of the housing and lack of variation in its roofline massing are not in character with the Back Bay, South End and St. Botolph St.

Residential edges. St. Botolph. Yarmouth. West Canton. A sympathetic treatment of edges visible from residential areas is critical to the preservation of neighborhood character. The design treatments as well as height should be appropriate to the scale of the residential areas. Facades of all buildings should be softened through elements such as glassed areas that give visual access to the activities within; high quality architectural treatment with appropriate materials; irregular setbacks or other modulation of the facade; landscaping; rooftop uses, etc.

In addition, Harcourt St. at the end of St. Botolph: the developer should provide a landscaped walkway (see pedestrian routes). Active use such as retail or office should be considered at the walkway levels (60' either side of the centerline of St. Botolph St.).

Any bridge over Stuart St. should avoid the feeling of a tunnel and allow air and light.

No part of the site on public streets should be treated as a back end without either active uses or visual interest.



FINDINGS

E. Pedestrian Environment:

Along most Copley Place edges, sidewalks are adjacent to massive, inactive walls rising more than 60' to the top of the podium. In this situation, widened sidewalks and landscaping are not enough to soften the impact. Unlike surrounding commercial and residential areas, there are very few entries and exits along Copley Place edges, and the use of large scale panels and few materials further reduce the variety and accessibility of public pedestrian spaces at Copley Place. The scale of the project's pedestrian environment is set by the car and by the internal plans of the project. Copley Place has missed the opportunity to re-link the adjacent neighborhoods.

RELEVANT GUIDELINES

Pedestrian links that are human scale, safe and active should be developed to Prudential, Copley Square and Back Bay.



MASSING

The major difficulties with the massing of Copley Place's present design are covered under discussion of scale. Other specific concerns are:

FINDINGS

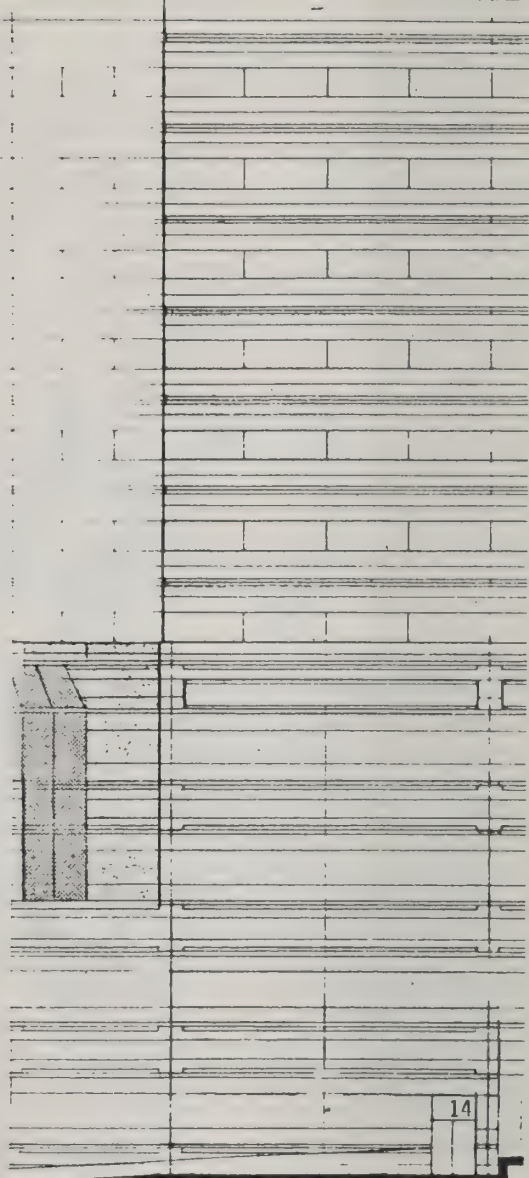
A. Broad faces of the tower portion of the Marriott Hotel are shown facing the South End and St. Botolph neighborhoods. This breaks the pattern of high towers along Huntington Ave., and imposes the high-rise elements on the medium to low rise residential areas. According to the Parsons Brinkerhoff drawing "Site Plan with Building Lines - 9.10.80, the Marriott tower is shown moved approximately 8 feet further back from the St. Botolph visual easement. This desirable change is not reflected in the "Final Design Plans" for the hotel.

B. The office tower facing onto the Southwest Corridor deck is too high, especially in light of its position next to the pedestrian areas of the deck, the existing South End housing and future Tent City housing. In converting much of the central gallery space to office space, the designers have taken the opportunity to vary and stagger spaces in the mall. In contrast the even reduction in height of all four office towers ignores the possibility of making the exterior office massing more varied.

RELEVANT GUIDELINES

3. Harcourt St. -- within the St. Botolph St. visual easement (extension of the line formed by the house fronts on the north side of St. Botolph St.) buildings along Harcourt St. edge must be no higher than 95' from the street.

Any building higher than 95' (such as office tower) should be kept north of the St. Botolph visual easement (see previous paragraph) and as far from that easement as possible.



FINDINGS

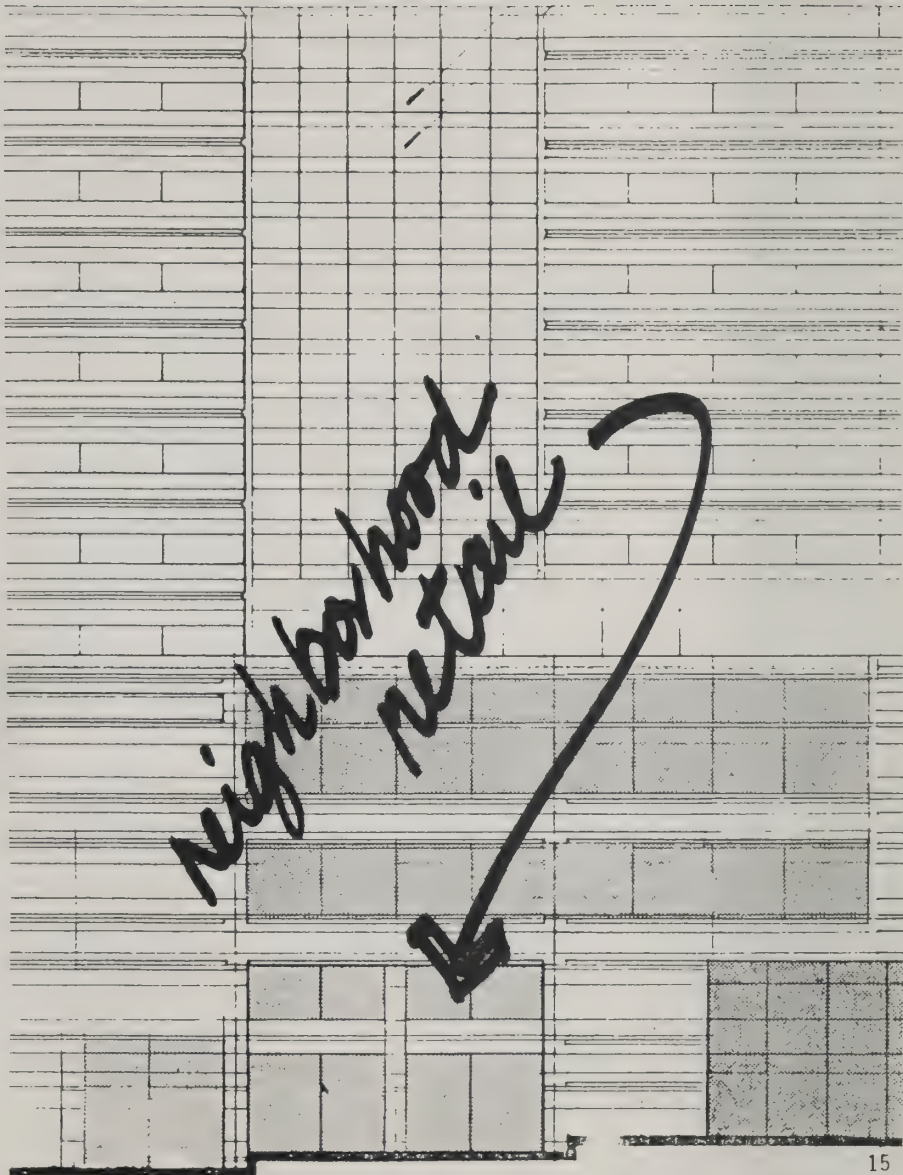
C. The neighborhood retail should have sufficient access and visibility to allow it to succeed commercially. Its floor levels should be the same as the mall entry and other major retail floors. Stepping down into the neighborhood retail, as shown in 9.11.80 plans, is not acceptable. The "Final Design Plans" do not show door locations for the neighborhood retail. Entries should be provided from both the mall entry and the SWC deck pedestrian area.

MATERIALS

As has been clear throughout the CRC process, use of appropriate materials is critical if the project is to fit into the city well.

This is particularly true to the top of the podium on all facades. Mineralite and other synthetic materials are not acceptable on these facades.

There is not enough variety in materials and in facade surface treatments used through the project.



FINDINGS

On facades near streets or pedestrian ways, materials should be varied and appropriate in scale to the activities adjacent to them. This is not done in Copley Place facades other than those of the Hotel Boston.

The Southwest Corridor facade is particularly inappropriate. The exposed aggregate precast concrete is not varied in treatment between the edge of housing and the neighborhood retail, a long facade next to a major pedestrian route and housing.

The facade of Neiman-Marcus along Dartmouth St. is also inappropriate; its scale and uniformity of treatment is not likely to be supportive of, and does not provide a desirable backdrop for, the Dartmouth St. mall

The entrance to the mall and neighborhood retail at Dartmouth and the Southwest Corridor Deck is a major part of linking Copley Place to surrounding developments and neighborhoods. It should be as significant as the other entries to the mall.

Transition between materials throughout the project, especially between the office and Marriott Hotel towers and the podium, should be handled carefully. Dry-vit or any other synthetic material is not acceptable as surfacing for the housing facades.

ACTIVE FRONTIAGES

Visible and/or accessible activities along the project's edges are essential to making the project fit the city and respond to the pattern of adjacent areas.

LEAST ACTIVE FRONTAGES



FINDINGS

Copley Place will bring space for roughly as many stores to its internal mall as Back Bay has along Boylston and Newbury Streets between Dartmouth and Arlington Streets. In part because most of Copley Place's commercial activity is invisible, and only indirectly accessible from the street, the project does not contribute to the vitality of the city the way Back Bay or Washington Street retail areas do.

There are several other areas where the guideline requiring maximum active frontages is not met:

A. The southwest corridor facade between the housing and neighborhood retail sections. Making the restaurants visible is a help, but treatment of the facade below the restaurants remains a problem.

B. The Marriott Hotel facade from the ground to the top of the podium, between Huntington Ave. and the edge of the housing. These two sections present relatively flat precast concrete or mineralite facades, approximately 90 feet in height, to the adjacent residential neighborhoods, which are generally 3-4 story brick townhouses. Adding brick to the lower facade

RELEVANT GUIDELINES

To make activity as well as physical links between the adjoining communities and the site.

To increase safety on the site and surrounding streets during the evening as well as during the day, active uses such as housing should be promoted.

To reinforce the existing character of the South End and Back Bay by constructing a mixed use commercial/residential complex and a related system of pedestrian, vehicular and transit improvements.

To reinforce mixed income residential character of the surrounding communities.

To complement and reinforce the commercial character of Prudential Center, Boylston/Newbury St. and not undermine the existing retail on Washington St.

FINDINGS

around the service entry reduces the jarring effect at the end of St. Botolph St., but the hotel functions remain hidden.

C. The entrance to the mall and neighborhood retail are weak compared to the other mall entries. This is inappropriate given that they face onto Back Bay station and are at one end of the mall and at the junction of two major pedestrian routes. That part of Neiman-Marcus adjacent to these entries is primarily storage, which does not contribute to the strength of the corner or the activity level of the project along Dartmouth St.

D. The four surface level housing units facing onto the Southwest Corridor deck are insufficient to provide activity or a feeling of housing in that area.

FORM

Impact on Residential Areas

The Southwest Corridor deck and Harcourt St. facades are not in compliance with these guidelines.

FINDINGS

The blank wall over the Marriott service entry is not acceptable as the primary view of the project from St. Botolph St.

The flat facades along the Southwest Corridor deck which face onto Dartmouth St. and the Tent City site are also unacceptable.

Wind Effects:

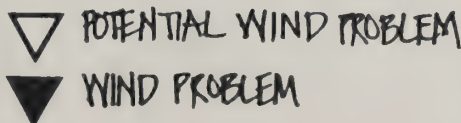
Several pedestrian-level wind problems were identified by the Bolt Beranek and Newman wind study at several points around the site. The "Final Design Plans" do not show treatments to counteract these problems, and is in violation of the guidelines. The following locations are of particular concern.

The massing and orientation of the Marriott Hotel place a major entry to the hotel, to the project, and two major pedestrian paths on Huntington Ave. in front of the hotel. This edge is already, and in the project design, rather hostile to pedestrians.

RELEVANT GUIDELINES

2. Negative visual impact of new construction on surrounding streets should be avoided. Particularly sensitive streets from which the development is seen are: Copley Square, St. Botolph, West Canton, and Yarmouth.

1. Tall or bulky elements of the project should be oriented and placed to minimize undesirable wind effects and increased shadows on adjoining properties and public spaces.



FINDINGS

All three corners of the Hotel Boston site are likely problem areas. The corner onto Copley Square exceeds the desirable wind speed limit, and winds at the other corners approach this limit. The extension of Dartmouth mall, the main entry to Hotel Boston, and the already difficult pedestrian crossings into Copley Square are all made worse by the project's wind impacts.

Two other sensitive areas have winds approaching the limit of unacceptability: the Southwest Corridor Deck near the entry to the neighborhood retail space; and the south side of Stuart St. near the Turnpike ramp exit.

Facade Treatments:

There are too many large blank walls along the podium. Using different materials, making activities visible, and changing the scale of facade elements would all help this problem.

PEDESTRIAN CONNECTIONS FINDINGS

RELEVANT GUIDELINES

The treatment of exterior pedestrian ways is, in most parts of the project, out of compliance with the guideline intention to rejoin areas of the city through the project, and to bind the development to the surrounding areas in a constructive manner.

A. South End to Copley Square along Dartmouth St.

1. The corner where the Southwest Corridor deck, Dartmouth Mall Extension and exit from Back Bay Station underpass come together needs coordinated, pedestrian-oriented design.
2. The crossings of Huntington Ave. and Stuart St. should be protected from high winds, made narrower and more inviting.
3. The mall width between the Neiman-Marcus facade and Dartmouth St. curb should remain at least 21 feet, landscaping should be arranged to avoid reducing the effective width.

One of the strongest potentials for this site is that of rejoining areas of the city separated by traffic and large scale development. The following pedestrian guidelines attempt to encourage linking of the site to adjacent areas and humanizing of conditions at ground level.

Certain major pedestrian routes around and through the site are necessary to responsive development in this location. Three of these are public ways which must be open at all hours, two are semi-public ways through the site. Any of the following routes can be realigned through the site as long as they satisfy the same criteria. Certain conditions apply to all routes:

1. Protection from excessive wind turbulence, noise and fumes must be provided for pedestrians on all routes. Routes must be clearly marked and continuous.
2. All the major public routes must be traversable by the handicapped.



FINDINGS

B. Back Bay Station to the Prudential Center via the Southwest Corridor Deck and Harcourt St.

1. The deck cover funding must be secured and designs coordinated between the SWC/MBTA and UIDC.

2. The ten housing units and one neighborhood store provided along this path do not provide enough activity to make the route attractive or safe.

3. The bulk of the project overpowers the adjacent housing along the deck and along Harcourt St.

4. The walkway along Harcourt St. from the end of the housing to Huntington must be buffered from service vehicles on both the UIDC site and on the access to Public Alley 401.

C. Harcourt St. to Copley Square via Huntington Ave.

Providing more sidewalk space in front of the Marriott is an improvement over earlier plans, but treatment of this route is unacceptable in several respects:

RELEVANT GUIDELINES

3. All routes should be well-lit when open and designed for safety (e.g. located adjacent to active uses, easy to patrol, etc.).

4. When semi-public paths are closed, they should be closed at points where there are clear alternative public paths.

5. Special care must be paid to the sections over the Mass. Turnpike especially Dartmouth St. and Huntington Ave. to ensure protection for pedestrians from pollution -- noise fumes, and visual. The preference is for active uses such as retail which will both provide activity and block out the Turnpike.

6. Clear and workable arrangements for developing, managing and maintaining pedestrianways must be arrived at by the participants -- the City, MBTA, developer.

1. Public. South End to Copley Square along Dartmouth. Extension of a major pedestrian route from the Public Library to Columbus Avenue is necessary in conjunction with development of the air rights site. This route has been improved, as the Dartmouth Street Mall, up to Boylston Street, and should be extended now to Columbus Avenue.

FINDINGS

RELEVANT GUIDELINES

1. There is not enough protected space for pedestrians along the route. The many curb cuts and excessively wide turns -- which are not consistent with the street pattern in surrounding areas -- leave the route discontinuous.

2. Signage has not been discussed, but it appears that this route along Huntington Ave. will be confusing for vehicles as well as pedestrians.

3. Strong winds along the route aggravate the problems.

D. Back Bay Station to Prudential Center through the project. (Semi-Public Route)

1. The pedestrian bridge to the Prudential is not a substitute for acceptable ground-level pedestrian treatment.

2. The route through the retail mall is not an acceptable substitute for active, continuous, attractive ground-level routes around the project.

2. Public. From South End, Tent City site and Back Bay station to the Prudential Center and Back Bay on the MBTA cover and Harcourt St. to Huntington Ave. It is essential that activities and route be designed and located to maintain safety, surveillance, and access to patrols at all times. The MBTA should design on the deck managed active areas such as community recreation and other measures necessary to make the route safe and inviting at night. Retail or other facilities fronting on the MBTA tract cover should provide good lighting and some visual access to inside activities.

3. Public. From St. Botolph along Huntington Avenue to Copley Square.

4. Semi-public. From St. Botolph St. through the site to Copley Square

5. Semi-public. From Back Bay Station, South End and Tent City site through the site to Prudential Center and Back Bay.



INTERSECTIONS

FINDINGS | RELEVANT GUIDELINES

These are areas of joint responsibility by the city, developer and other public agencies. It is time for the public sector designs and improvement funding to be made public and secured in order to ensure that Copley Place improvements will be supported as planned.

The realignment and narrowing of Stuart St. adjacent to the Hotel Boston is an improvement over the present pedestrian pattern.

Problem Intersections:

1. Across Huntington Ave. and Stuart St. along Dartmouth: The wind problems created by Hotel Boston make these wide roads more difficult to cross.

2. From Harcourt across Huntington Ave. to the Prudential: Huntington Ave. is now difficult to cross, and Copley Place does not improve this. The Prudential bridge, which may not be open 24 hours, is not an acceptable substitute for an accessible ground level crossing.

The intersection of St. James, Huntington, Blagden, and Dartmouth must be made less hostile to pedestrians. Improvements such as crosswalks, new surface materials, curb cuts, signal adjustments for increased pedestrian crossing time, plantings, clear signage, increased sidewalk width, street furniture, and other special features are primarily the City's responsibility. It is necessary, however, that these changes be closely coordinated with the developer's improvements. It is also important that the changes are designed to be consistent with traffic flow necessary to maintain safety on the Mass. Turnpike.

Another joint responsibility crossing is that from the future reconstructed Back Bay Station across Dartmouth to the air rights site. This connection provides access to 3 major routes: Dartmouth Street route, path on the future MBTA track cover, and the path through the site to the Prudential. Like the other intersections, treatment must be coordinated; in this case by the City, air-rights developer, and designers of the new Back Bay Station.



FINDINGS

3. The crossing of Stuart St. at Huntington is part of a major pedestrian route and needs special paving or a pedestrian light.

RELEVANT GUIDELINES

Treatment consistent with that of the above intersection should be used to aid crossing Stuart Street at Dartmouth and at Huntington. These crossings are again primarily the responsibility of the City and must be coordinated with the developer's designs.

RECOMMENDATIONS

RECOMMENDATIONS: OVERVIEW

Five sets of drawings were presented as "Final Design Plans" to the Design Review Subcommittee on September 11, 1980. The Subcommittee's recommendations are keyed to the sets of drawings, although they are at quite different stages of finality and detail.

Three types of recommendations are made:

1. Recommendations to approve or disapprove particular design elements. Items not listed for specific approval or disapproval, and not listed under either of the other categories of recommendations should not be assumed to be automatically approved. The recommendations and findings identify the positive and negative highlights of the design. The findings of the committee regarding the character of the design in relation to the guidelines should guide the MTA in its deliberations.

2. Recommendations to recall the committee if changes are made in listed items which are critical to the design. Major changes of any type, even if not specified here, should be presented to the committee for review.

3. Recommendations that the MTA withhold approval of certain design aspects until more detail and greater certainty are available for design review.

On the basis of the "Final Design Plans, 9.11.80" and subject to fulfillment of other recommendations, the MTA should:

1. Approve the general location of program elements on the site.

2. Approve the tower heights.

3. Not approve use of synthetic cladding materials on any part of podium facades.

4. Not approve the design of the exterior pedestrian paths and surrounds.

<u>BUILDING</u>	<u>DESIGN PHASE</u>	
		<u>MATERIALS PRESENTED AND LAST REVISION DATE</u>	
Hotel Boston	Late Design Development	Plans, sections, elevations, wall sections	8/11/80
		Specifications	6/16/80
Central Mall (including pedestrian bridges)	Early Design Development	Plans, sections, elevations, wall sections	9/4/80
		Outline specifications, office tower only	7/11/80
Marriott Hotel	Schematic	Plans, elevations, section	9/9/80
		Specifications	6/15/80
Housing	Sketch Design	Site plan, diagrammatic plans & section, elevations	9/1/80
		no specifications	
Landscaping	Conceptual Sketches	Sections, plans SMC Deck and Huntington Ave.	8/29/80

RECOMMENDATIONS:

HOTEL BOSTON

The MTA should:

1. Approve the location, height and massing of the hotel.
2. Approve the use of glass "sheds" along the hotel's Dartmouth St. facade.
3. Approve the width of Dartmouth Mall extension (21 feet min.) from Huntington to Stuart St.
4. Not approve the elevation facing Copley Square and ask UIDC to redesign it to fit better with the scale of other building facades on the square.
5. Not approve the treatment of the pedestrian path along the Huntington Ave. edge of the hotel until pedestrian connections across Stuart St. and Huntington Ave. are improved.

Re-initiate Design Review if any changes are made in:

1. Any of the items listed in these recommendations regarding the building.
2. Any major changes in the building or its site plan.

Withhold approval of the project until more detail or final decisions are available regarding:

1. Exterior cladding, paving and glazing materials.
2. Treatment to reduce pedestrian wind impacts at all corners of the triangle site. Any such treatments should not reduce effective sidewalk widths.

RECOMMENDATIONS: CENTRAL BLOCK

The MTA should:

1. Approve the location of program elements in this part of the project.
2. Approve the minimum 21 foot width of the Dartmouth Mall extension.
3. Approve the height of the office towers except the one facing onto the Southwest Corridor Deck. This tower should be lower.
4. Not approve the design of the entry to the neighborhood retail until this is made more prominent.
5. Not approve that portion of the facade onto the Southwest Corridor Deck between the housing and neighborhood retail until the scale of the facade is made more humane.
6. Not approve the Neiman-Marcus facade along Dartmouth Mall until it is made more active and the scale of materials is made more humane.

Re-initiate Design Review if any changes are made in:

1. Vehicular or pedestrian access to the block.
2. Any items listed in these recommendations.
3. Any major changes in the design.

Withhold approval of the project until more detail and final decisions are available regarding:

1. Exterior cladding, paving and glazing materials.
2. Landscaping on all edges of the mall.
3. Final massing of the block.
4. Design, funding and construction scheduling for the Southwest Corridor Deck and the Back Bay Station Underpass.
5. Access to and layout of the neighborhood retail space.
6. Signage along the Huntington Ave. edge of the block.

7. Approve the use of a glass entry pavilion at the Dartmouth St. Mall entry, and require that the neighborhood retail be accessible from it.

8. Not approve the design of the pedestrian path along Huntington Ave.

9. A sidewalk should be provided on both sides of Stuart St.

RECOMMENDATIONS: MARRIOTT HOTEL

The MTA should:

1. Approve the location and height of this part of the project subject to successful fulfillment of other recommendations.

2. Not approve the massing of the hotel until pedestrian level wind problems are successfully treated.

3. Approve moving the hotel tower further back from the St. Botolph St. visual easement and require that this change be made in the building plans.

4. Not approve the treatment of the Hotel's Harcourt St. facade until its scale and activity are made more sympathetic to the St. Botolph St. area.

5. Not approve the design of the pedestrian areas along Huntington Ave. and Harcourt St. until the pedestrian paths are made clearer and the pedestrian environment more humane.

Re-initiate Design Review if any changes are made in:

1. Massing of the hotel tower.

2. Any items listed in these recommendations.

3. Any major changes in the hotel

Withhold approval of the project until more detail and final decisions are available regarding:

1. Exterior cladding, paving and glazing materials.

2. Treatment to reduce pedestrian level winds along Huntington Ave.

3. Landscaping of Harcourt St. and Huntington Ave. pedestrian paths.

RECOMMENDATIONS: HOUSING

The MTA should:

1. Approve the number of units.
2. Not approve the plans until the housing is extended further toward Huntington Ave. and toward Dartmouth St.
3. Not approve the massing of the housing until the height is varied along the housing facade.
4. Not approve the housing until more units have ground-level access.
5. Not approve use of synthetic materials on the housing facades.

Re-initiate Design Review if any changes are made in:

1. Any of the items listed in these recommendations.
2. Any major changes in the housing design.

Withhold approval of the project until more detail and final decisions are available regarding:

1. Design, massing and landscaping of the housing.
2. Exterior cladding and paving materials.

RECOMMENDATIONS LANDSCAPING

The MTA should:

1. Give preliminary approval of the landscape plans along Dartmouth Ave and the Southwest Corridor Deck.
2. Not approve landscape plans along Huntington Ave. and Harcourt St. until: a) the feasibility of current proposals as evaluated by City, MTA and MBTA engineers; b) the pedestrian path along Harcourt St is adequately buffered from service vehicles; and, c) the pedestrian path along Huntington Ave. is made more active and humane.

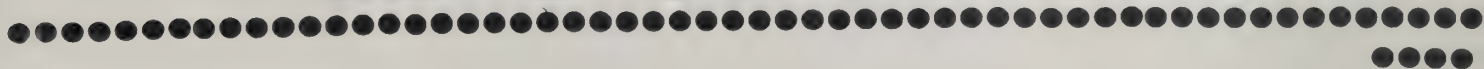
Re-initiate Design Review if any changes are made in:

1. The funding status of design of the Southwest Corridor Deck, public sidewalks adjacent to the site, and the pedestrian bridge to the Prudential.

Withhold approval of the project until more detail and final decisions are available regarding:

1. Paving and plant materials.
2. Coordination of UIDC landscape plans with City and Southwest Corridor landscape and pedestrian treatment plans.
3. Final design of the landscaping.

APPENDICES



..... "11.1 Tenant will construct on the Demised Portion a number of buildings and other improvements in accordance with Tenant's draft and final environmental impact reports and the comments of the Secretary of Environmental Affairs thereto, of which the principal elements will be hotels, office buildings, an apartment structure, and department stores, together with related parking facilities, stores, shops, restaurants, theatres and sports facilities. In erecting such buildings the Tenant shall be entitled to the support provided in Article IV and Tenant agrees that the construction of such buildings shall be in accordance with the terms of Schedule C attached hereto and made a part hereof. Tenant shall furnish to the Landlord and Landlord will furnish to the Design Review Subcommittee of the Citizens' Review Committee (herein called the Design Review Subcommittee) established pursuant to Schedule C of this lease, the plans and specification for the proposed buildings and appurtenances thereto. It is contemplated that Tenant will furnish to the Landlord preliminary plans and specifications, and that the Landlord will make the same available to the Design Review Subcommittee and, with the advice of the Design Review Subcommittee, will offer suggestions and establish requirements for changes and additions to the plans and specifications, which changes and additions will be incorporated into revisions of plans and specifications until, by a continuous process of revision and review of plans and specifications, final plans and specifications are submitted by the Tenant to the Landlord and by Landlord to the Design Review Subcommittee. Landlord, with the advice of the Design Review Subcommittee, shall have thirty (30) days from the date of delivery of such final plans and specifications to approve the same or indicate in writing to the Tenant its specific objections thereto. In the event of such a disapproval by the Landlord of the Tenant's final plans and specifications, (i) the Rent Increase Date, (ii) the effective date of cancellation after any such notice shall be extended by an amount of time equal to the time required by Tenant for revision of plans and specifications required to meet Landlord's objections.

Review of Project Plans and Specifications

11.4 With respect to any plans and specifications submitted by Tenant to the Landlord and by Landlord to the Design Review Subcommittee pursuant to the foregoing provisions of this Article XI, if the Landlord shall disapprove aspects thereof relating to the design of the proposed construction for any reason, including conflict with the Guidelines contained in the Final Recommendations for Copley Square submitted by the Copley Square Citizens' Review Committee on September 22, 1977 referred to in Schedule C of this lease, but excluding reasons relating to the safety, maintenance, operation

or repair of the Turnpike (as to which the Landlord shall reserve discretion, subject only to the provisions of Section 14.11) then Tenant may submit the matter of reasonableness and propriety of such disapproval to a board of review comprised of three architects duly licensed to practice in Massachusetts and members of the American Institute of Architects, one to be selected by Landlord, one to be selected by Tenant and one to be selected from its list of experts on architectural design by the Boston office of the American Arbitration Association, or its successor. If Tenant selects a member of the board of review and Landlord fails, within ten (10) days after the request of Tenant, to select a member, then the decision as to the reasonableness and propriety of such disapproval of Landlord shall be made solely by the member chosen by Tenant; otherwise, such decision will be made by a majority of said board. The decision of said board shall be final, conclusive and binding upon Landlord and Tenant. In making its determination such board shall be authorized to take into account the effect of the proposed design of such construction on the overall area effected thereby and on other related air rights construction, present or proposed, along the Turnpike; provided, that such board shall not substitute its own particular design concepts in making a determination, but shall make such determination based upon whether the design proposed by Tenant is within the limits considered reasonable and acceptable by architectural theory and practice at the time of such submission; the purpose of such board being to avoid the possibility of unreasonable disapproval by or on behalf of Landlord in the future. If Landlord fails to approve or disapprove of any such plans or specifications within thirty (30) days after the submission or resubmission thereof to Landlord pursuant to this Section 11.4, such failure shall, for all purposes of this lease constitute approval by Landlord, provided that Tenant has used every reasonable effort to so submit or resubmit such plans and specifications in serial fashion so as to afford Landlord the opportunity to review, as soon as reasonably possible, such separate component of the proposed construction design." from Amended Lease.



MEETING SUMMARIES

Summary of Meeting
Copley Place, Design Review
Subcommittee
July 17, 1980, Back Bay Room,
Prudential Tower
Project Orientation

1. Tunney Lee opened the meeting and reviewed the agenda. The Subcommittee will be chaired by Tunney Lee, or by Ed Saunders, MTA, in his absence. A series of meetings will be held to examine aspects of the final project design.

1.2 The lease sections describing the subcommittee's mandate were reviewed. The Subcommittee must, as a group, supplement the published design guidelines with its own judgement as to the suitability of the design. Since the major tenants are committed to the project, it is possible to comment on the final design proposals.

1.3 The series of meetings will cover the following topics, on approximately the following dates. All meetings except the first and last will be held at the offices of The Architect's Collaborative, 44 Brattle St., Cambridge. All meetings will begin at 5:30 p.m.

July 17 - Orientation
July 24 - Massing
July 31 - Massing

Aug. 7 - Wind and ground level conditions, energy
Aug. 14 - Materials, surface treatments
Sept. 4 - Pedestrian ways
Sept. 11 - Project edge treatments, housing

After discussion, it was agreed to exchange the housing and materials meetings, so the revised schedule calls for

Aug. 14 - Project edges and housing
Sept. 11 - surface treatments and materials

A draft series of comments and recommendations will be prepared for presentation to the full CRC by Sept. 18. After review by the committee, the report will be submitted to the MTA.

1.4 The committee's report will be recommendations for change or acceptance of the design, and the Authority for requiring these changes remains with the Landlord - i.e. the MTA. The committee will continue to function until the working drawings for the project are complete. The BRA design review group will continue to monitor the project after that time.

1.5 This design review process is connected to the review required for UDAG approval.

Bob Kroin, BRA, reviewed the

status of the UIDC submission for UDAG review. The material received by the BRA is incomplete, additional information and drawings have been requested. The present process is considered "design development" review, the problems with the design are the same as those in last year's design. The BRA will be looking for solutions to them in the present proposals. The design is also subject to Traffic and Public Works Commission review. The tax agreement for the development is affected by the design review.

1.6 The Subcommittee will concentrate its review on the exterior design of the complex and its relationship with surrounding urban areas. Pedestrian access to and through the complex will be considered, as will vehicular servicing of the project, and housing.

1.7 Copies of the drawings submitted to the MTA for review, as well as a complete set of reports prepared during the review process, will be made available for committee members' review at TAC, and at Tunney Lee's office, (25 Union St., Boston, 367-9081) Deborah Poody will serve as staff to the committee.

2.0 Bob Kroin reviewed the BRA's concerns, which are based on the material they have received to date.

a. They have no information on the housing, the project will be unacceptable until the housing is defined and approved.

b. Large areas of the project are not defined as to use, this is unacceptable.

c. They consider the Dartmouth and Stuart street facades inactive, as presently designed, and would prefer some retail uses there.

d. The Dartmouth Street mall should clearly be continued along Dartmouth, not starting and stopping unnecessarily.

e. Wind is a major concern, it appears that 40 m.p.h. winds will be generated at the corners and entries of the project, making the surrounding sidewalks unpleasant/unusable for pedestrians.

f. Building facades are not defined as to articulation, material, scale, material or amount of glass; shape of the Harcourt St. hotel also needs discussion.

g. The debate about the treatment of the ramp exit onto Stuart St. continues. The city's position is that pedestrians should be accommodated on the air-rights side of Stuart St., with a pedestrian-activated

crossing light at the ramp exit. The Turnpike Authority feels that this will interrupt the free flow of traffic from the ramp, and does not want a sidewalk on that side of Stuart St. The issues will be resolved by meetings between R. Ryan and J. Driscoll.

2.1 Ken Kruckmeyer and Dan Ocasio reviewed concerns relating to the SWC and MBTA.

a. The project entry off Dartmouth St. is still sterile, with only storage for specialty stores, more activity is desired.

b. The neighborhood retail does not have a street address, faces only onto the SWC deck, and is very likely to fail. Second floor neighborhood retail has even worse access and less chance of success.

c. The housing, as presently drawn, cannot be accommodated by the SWC deck. To work with the deck, the housing would have to move back 12' from its present edge. This may be a serious problem.

d. The housing as presently shown is not in scale with the adjacent neighborhoods. The arcades previously shown seem to have disappeared.

e. The SWC group is working on construction staging for the

deck. Accomodating Copley changes will cost an additional \$3 million. This will pay for: (1) moving the B & A tracks now, rather than in the future, (2) providing an underpass from Back Bay Station to Copley Place, (3) developing a permanent rather than temporary deck over the mainline tracks, and (4) the underpass to the project is still a subject of debate. The MBTA prefers that the pedestrians exit into the project, rather than onto the public street in front of it. The present plans do not show the Copley Place entry coming out far enough to include the exit. The MBTA does not want to build the underpass unless the design is changed to allow the connection to exit into the project. This is basically for security reasons.

3.0 After the main CRC meeting the group reconvened.

3.1 Better information is needed for the committee's review - i.e., 1/8 and 1/16 scale drawings of the project, including elevations.

3.2 The September meetings were moved back to August dates.



Summary of Meeting
Copley Place, Design Review
Subcommittee
July 24, 1980
TAC
5:30 p.m.

The architect gave a quick overview of the massing of the project, referring to the model. He explained how the Copley Square frontage has been designed to carry through the lines of the Boston Public Library and the Copley Plaza Hotel. To effect this, glass sheds are used along the Dartmouth Street facade to add a layer of massing detail, and the tower of the Hotel Boston has been moved down Huntington Ave., so that it is well set back from the corner of Huntington and Dartmouth.

Both towers have stayed within the height limits of the guidelines; the Marriott hotel is placed so as not to block the St. Botolph St. visual easement.

The committee raised several concerns in response to the presentation. Many felt that there were problems with the Marriott hotel. The western wall presents a solid mass toward the South End, and this needs to be softened and relieved in some way.

A considerable amount of time was spent discussing the massing and orientation of the hotel. The

Marriott hotel tower has a broad massive side facing the South End and a narrow, receding side, facing Huntington Ave. Many believed that this orientation should be reversed, as Huntington Ave. is a much more appropriate place for a massive facade.



Summary of Meeting
Copley Place, Design Review
Subcommittee
July 31, 1980
TAC
5:30 p.m.

This meeting continued the massing discussion started at the July 24 meeting.

The architect explained the massing of the projects in more detail. Beginning with the Western Hotel, he explained that the base was designed to carry the lines of the Boston Public Library and the Copley Plaza Hotel. The plans call for shifting Stuart St. to enlarge the triangle site. This makes it possible for the Western Hotel tower to be substantially set back along Huntington Ave. The entrance to the project through the Western Hotel base has been created by faceting the corner. This faceting is echoed in the tower itself. Shifting Stuart St. has also increased the sidewalk space along the Dartmouth St. Mall.

The discussion then turned briefly to materials, focusing mostly on the semantics of masonry and precast. The project is expected to be clad in exposed aggregate precast concrete and smooth finish precast.

The architect then discussed the massing of the offices and the retail block. Although a third tower was once contemplated here, the mass of the offices has been kept horizontal instead.

In the ensuing discussion, several concerns were voiced.

Some feel that the policy of accomodating the needs of automobiles before those of pedestrians is inappropriate and that the community is adversely affected by the design. Specific reference was made to the almost solid wall that the project presents to the South End.

The architect responded that this part of the design was forced to accomodate the restraints of the Turnpike ramp.

Discussion focused for sometime around the treatment of the Dartmouth St. Mall. There is a general feeling that the developer was showing a lack of enthusiasm in designing this section of the link between the South End and the Back Bay. Specific concerns included the narrowness of the sidewalk along the mall, and the projection of the Neiman-Marcus store into Dartmouth and Harcourt St. The projection should be cut back at least to the point of not blocking th visual easement from the South End to the Back Bay.

Discussion returned to the massing of the Marriott hotel. The committee would like to see the mass flipped or turned to exchange the facades now presented to Huntington Ave. with those facing the South End.



Summary of Meeting
Copley Place, Design Review
Subcommittee
August 7, 1980
TAC
5:30 p.m.

The meeting opened with a statement from the Subcommittee suggesting a revision of procedure, such that the meetings with the architect and developer be used as information meetings. The Subcommittee will meet together and respond as a group to the design. It is hoped that this will enable a comprehensive review of the project.

The rest of the meeting was devoted to geology, wind, shadow and energy findings, as they affect design.

Haley and Aldrich are the foundation engineers for the project. They explained the results of their soil tests and ground water monitoring. The project will require piles to bear the foundation, and they have recommended pre-augering of most pile holes in order to limit the noise and ground vibration associated with pile driving. Excavation is being kept to a minimum, generally 10 to 25 feet below the existing surface. This will have little impact on adjacent streets and property. Only a small amount of dewatering

will be required, limited across most of the site to pile caps; it is expected to have an insignificant impact on ground water levels.

Bolt Beranek and Newman have been responsible for the wind studies for the project. Leonard Fortier presented their findings. There are already wind problems in Copley Square the Copley Place Development will not severely increase or decrease these. The masses of the Marriott and Western Hotels have been kept low and fairly stable. Potential problems along the facades have been relieved by the great number of softened corners in the design.

BBN recommends using wind screens to alleviate the severe pedestrian level wind problems near the entrance to the Western Hotel.

The little Prudential tower creates significant pedestrian level wind problems at the Marriott hotel entry. This problem is dealt with by having pedestrians use an alternative route, under partial cover, through the Marriott taxi drop off.

The architect then presented the findings of the study of shadow impacts on the surrounding neighborhood. Shadow patterns were projected several times a day for each season. Considering the already substantial patterns of

shading the Copley Place development will have minimal impact. Shifting the Western Hotel tower has alleviated most shading of the Boston Public Library.

Finally, a presentation of energy policies and impacts was made by William Byrne of UIDC. State codes set thresholds of energy conservation for new construction, and the Copley Place project will stay within these limits. The decision was based on three guidelines, 1) economic operation, 2) ease of maintenance, repairs and replacement, and 3) capital cost of acquisition and installation. They recommended that the project be all electric, with the exception of gas boilers for heating process water for the hotels and restaurants. The bulk of the electric usage will be during off-peak hours.



Summary of Meeting
Copley Place, Design Review
Subcommittee
August 18, 1980
TAC
5:30 p.m.

This meeting first discussed materials, and then of pedestrian and housing issues, using the new 1:20 scale model.

There was discussion of the definition of "final plans" as concerns the lease and the involvement of the subcommittee in review of any changes subsequent to the "final plans". The committee feels that it should be convened in the case of any such changes. A formal statement on the issue will be forthcoming from the lawyers.

The architect then presented the materials that may be used in the project. Brick paving will be used along Dartmouth Mall and other pedestrian passages around the project. Precast concrete will be used, although the specific aggregate has not been chosen. Two kinds of glass will be used: clear glass for all of the base elements and the two hotel towers; and the tinted strictly for the office buildings. Materials used behind the clear glass (draperies etc.) will be controlled.

Brick for the housing is being chosen to match the adjoining houses. They are also discussing using dry-vit, a mineral spray-coated polystyrene.

Several types of precast are being considered and TAC is experimenting to find a way to get a warm effect. The architects are considering two aggregates, one, crushed milford granite, is the same stone used in the Boston Public Library. The other is a buff stone from Canada.

They also want to combine, in layers, a rough and a smooth treatment of the precast, as contrasting elements to break the scale.

The architect referred to both material samples and architectural renderings to explain their plans. The point was made by a committee member, that the renderings of the Boston Public Library were incorrect. The committee was concerned that decisions on the materials treatment were only in a tentative stage, and pointed out the need to review the final decisions.

The architect then explained that they might, because of cost considerations, not be able to use precast, in which case, they would use either mineralite or glass fiber-reinforced concrete. Panels would be expressed by rustification strips.

The durability of the non-precast materials was questioned, as well as the basis of the decision to reject glass or metal sheathing.

The architect responded that the cost of a quality metal and glass system would be high. Furthermore, the texture of metal would not be appropriate.

The architect then explained the landscaping plans. Brick laid in a diagonal pattern will be used on all the edges to tie the pedestrian area together. These will be separated from the building face by a granite or precast band.

Trees will be in circular planters, and bollards may be used to protect the pedestrians from the vehicular movement.

Designs were presented for the Southwest Corridor Deck landscapings, although no money has been budgeted for that work.

There was concern on the part of the committee that landscaping quality would be cut when the costs came in, replacing proposed 40 foot trees with 10 foot trees; landscaping on the deck is a joint UIDC and MBTA responsibility.



Summary of Meeting
Copley Place, Design Review
Subcommittee
Aug. 27, 1980
UIDC
5:30 p.m.

The meeting discussed pedestrian and housing issues.

The architect reviewed the network of pedestrian movement through the project, referring to drawings showing the connections at, above and below grade.

The design is responsive to the guidelines for access around and diagonals through the site.

Linking the central block to the west and south perimeter was discouraged because of concern about the parking impact on the St. Botolph neighborhoods.

At-grade movements are treated in four parts. (1) The treatment of the pedestrian crossings from Copley Square to the site has not been resolved. (2) Location of sidewalks and pedestrian crossings between the north and south sides of Stuart St. have not been resolved. This path is complicated by traffic of the Turnpike exit ramp. The committee would like to know what measurers would be used (street furniture etc.) to define the north side of

the street at the pedestrian path, and feels that the south side should have a sidewalk. (3) Movement along the Southwest Corridor Deck will come across from the Back Bay Station at grade, moving up Harcourt St. along the face of the housing on the east side. This path will be landscaped, with a wall as a buffer from the service entrance into the central block.

Below grade connections include the Dartmouth St./Southwest Corridor Tunnel.

The primary movement will be from Back Bay Station, up two escalators, along the principal diagonal of the project and onto the bridge to the Prudential Center.

The committee raised concerns in response to the presentation. The internal pedestrian way will most likely operate on a 16-18 hour day.

(4) Concern was also expressed about the frequent curb cuts along Huntington Ave. Drop-off circulation for the Marriott Hotel contributes to the hostile pedestrian environment along Huntington Ave. It should be possible to redesign this path in a matter more accommodating to pedestrians.

Julia Smith of Vitols Association gave a presentation on the housing

design. This part of the project will serve both as housing and as a transition between the scale of Copley Place and the residential neighborhoods of the South End. The ten story unit is divided into 3 bands. The top and bottom two stories are townhouse units. The intermediate floors are one and two bedroom flats. This creates a banding effect and ties the project to the horizontal character of the rest of the project. Bays in the facade further reflect the scale of the surrounding buildings.

The housing faces Harcourt St. and the Southwest Corridor. Only three townhouse units open to the Southwest Corridor Deck.

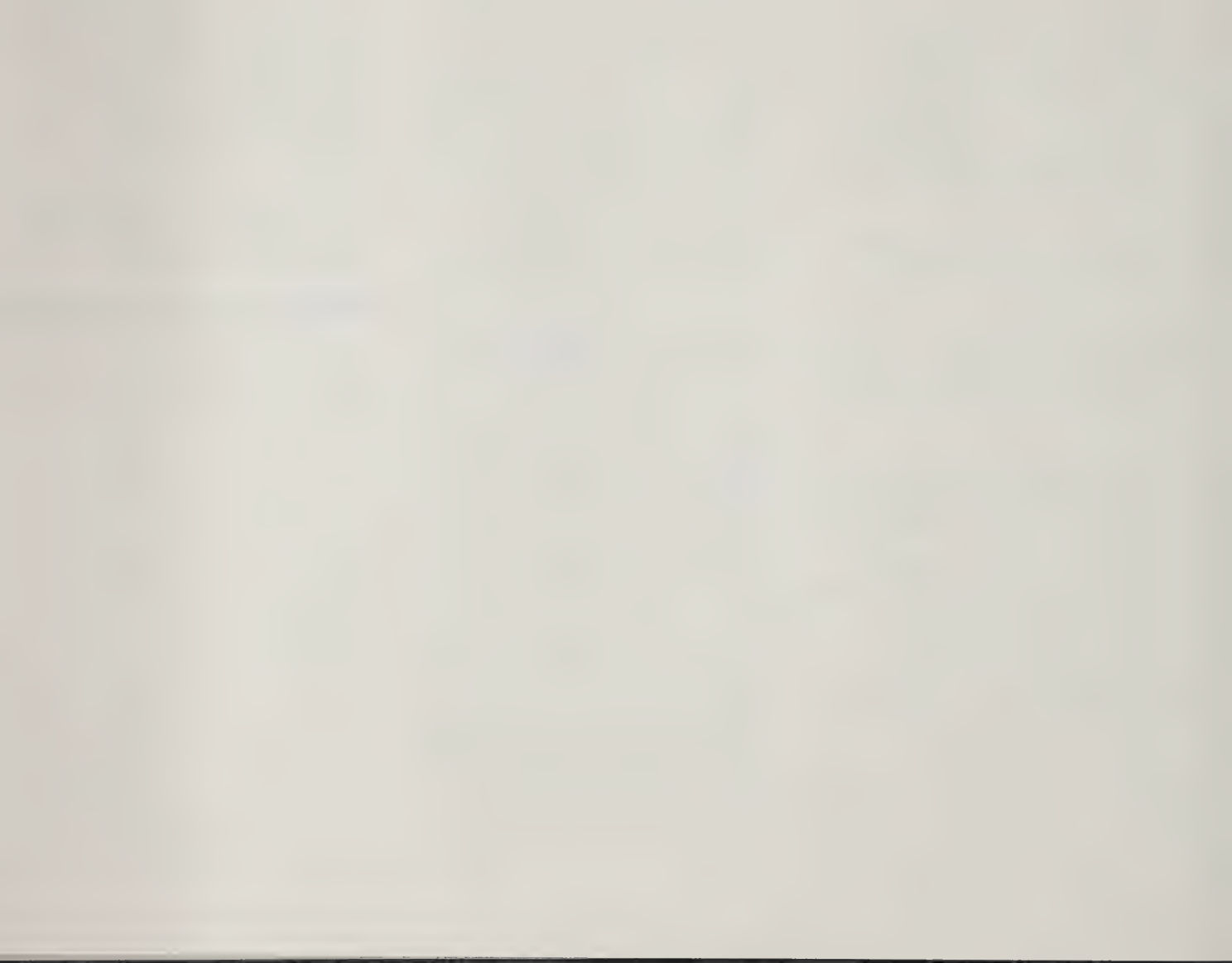
The townhouses are recessed on the lower floors to create a private yard. The penthouse townhouses have also been recessed to create private terraces, and to reduce the apparent height of the complex. Plans are to face the building in either brick or dry-vit, a brick substitute.

The committee was concerned that the height of the housing seemed to be out of scale with the rest of the neighborhood. To solve this, the committee suggested lowering and extending the housing along the Southwest Corridor edge, and toward St. Botolph St. More housing on the deck would give more activity than provided by with only three townhouse units.

Ms. Smith responded that this was not feasible because the fan and machine rooms already occupy that end of the main complex, and can't be relocated. Extending the housing along Harcourt St. was rejected because of cost and the high traffic on the service ramp makes this an unattractive location.

The committee questioned further the positioning of the fan rooms. The meeting ended without resolving the controversy.





Summary of Meetings
Copley Place, Design Review
Subcommittee
August 28, 1980
TAC
5:30 p.m.

This meeting was devoted to the architects' and developer's response to the statement of concerns presented by the subcommittee: "Preliminary reaction to the architects and developer's presentation at the last meeting".

1. The architect expressed commitment to the promotion of pedestrian activity and to "soft" edges throughout the project.

2. The architect explained that the project is not designed with a front and a back, but with many sides. A commercial side relates very directly to Copley Square and the Prudential Center. The residential side, softened by the Copley Place housing relates to the South End/St. Botolph St. neighborhood.

3. No response was given to the claim that the project is inward turning, giving priority to cars over pedestrians and the community.

A. Pedestrian Ways

1. The architect explained that the width of the Dartmouth

St. Mall will, in fact, vary from 21 to 25 feet wide.

2. Along the Huntington Ave. edge: a) the wind level at the pedestrian bridge depends on the Prudential's treatment of the bridge placement; b) the character of the pedestrian environment along Huntington Ave. will be improved by tree plantings; c) it is possible that the radii of the vehicular turns can be reduced. This will not be certain until after further traffic and engineering consultations.

B. Entries and Exits

The architects are now redesigning the Southwest Corridor entrance to the project, with a substantial cover and element moving out into the Southwest Corridor alignment.

1. The landscaping treatment around the neighborhood retail entrance has been revised, eliminating the planting buffer, and reducing the amount of planting in general, so that the entrance is no longer blocked.

2. The Neiman-Marcus and the plaza entry mall will receive humanizing treatment, with flags, flowers, landscapes, and a central element, possibly a sculpture.

C. Facade

1. The Hotel Boston facade

echoes the rhythm of the BPL, via the use of banding and layering of facade elements.

2. The Harcourt St. facade is not broken by windows because of its "back of the house" function which makes windows inappropriate. The use of brickwork to break up the face may be possible.

3. The concerns over signage were not discussed.

D. Materials

The committee's concerns over materials were not discussed.

E. Scale and Massing

1. The massing of the Marriott hotel is inhibited by several unfortunate constraints, making it difficult to manipulate the tower's massing.

2. Lowering the mass of the office building along the Southwest Corridor Deck would push up the mass on the other side. It is possible that the height of the entire office complex may drop in conjunction with a redesign of the gallery space.

F. Housing

1. The architects are considering the possibility of dropping the height of the housing

from ten to only nine stories.

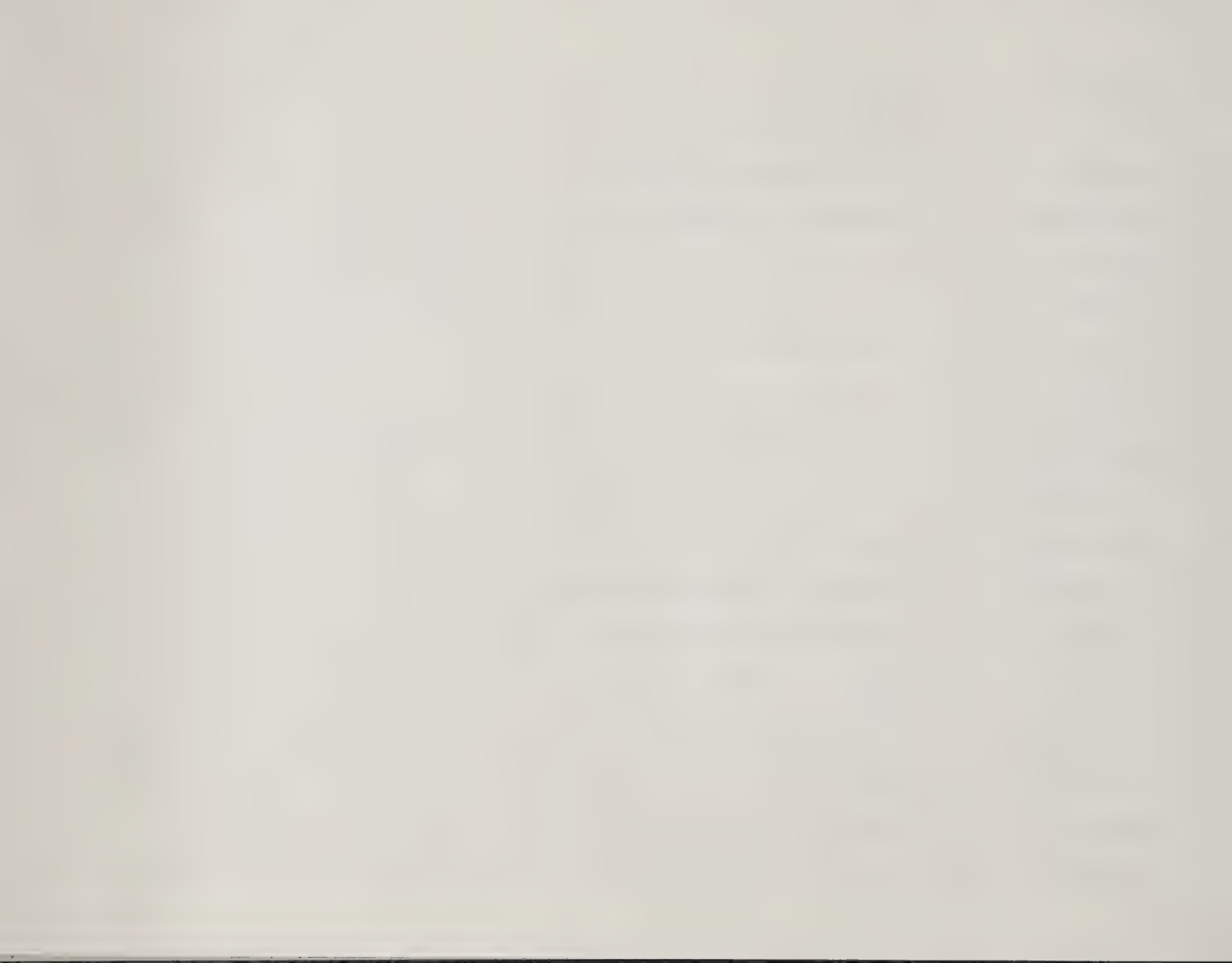
2. The first level of housing was recessed in order to create a private yard for these lower level townhouses without reducing space in the other apartments.



ATTENDANCE

	July 17	July 24	July 31	August 7	August 18	August 21	August 28	Sept. 4	Sept. 11
Ms. Berey	*								
Mr. Blakley						*			
Robert Bradley		*	*	*	*			*	
Steven Brovitz		*							
Joe Brown			*	*		*	*		
Mrs. Budenhogen					*				
Bill Byrne				*	*				
Norman Byrnes						*			
Jeff Chmura			*		*	*			
Dan Coolidge		*	*	*	*	*			*
Jeff Dando					*				
Malcolm Davis	*	*	*		*	*	*	*	
John Dugan				*					
Steve Eimer		*	*		*	*	*		
Howard Elkus		*	*	*	*	*	*		
Leonard Fortier				*					
Fred Greenwood		*	*	*	*	*		*	*
Ken Gritter	*	*	*	*	*	*	*	*	*
John Harrell									*
Ken Himmel		*	*		*				

		July 17	July 24	July 31	August 7	August 18	August 21	August 28	Sept. 4	Sept. 11
Janet Hunkel	Southwest Corridor KE/EST		*	*	*	*	*	*	*	*
Ed King	Mass. Turnpike Authority	*		*	*	*	*	*	*	*
Robert Kroin	Boston Redevelopment Authority	*								
Ken Kruckemeyer	Southwest Corridor/MBTA Architect	*			*					
Mr. Lacy							*			
Mr. Lasher							*			
Tunney Lee	Project Consultant	*	*	*	*					
Mr. Lee							*			
Julia Smith	Vitols Assoc					*	*	*		
Malcolm Tichnor	TAC		*	*	*			*		*
Ms. Uytterhouver					*	*				
Terry Weidener	UIDC		*	*	*	*	*	*		*
Mace Wenninger	Back Bay Architectural Commission									
Joan Wood	South End Project Area Committee	*	*			*	*	*	*	*
Hank Zuba	UIDC						*	*		
Rudy Umscheid	UIDC							*		
Steve Rosenfeld	Marriott Hotel Corporation							*		
Gordon Hislop	UIDC							*		
Nelson Hamener	Landscape Architect, TAC							*		
Bill Rogers	TAC							*		



		July 17	July 24	July 31	August 7	August 18	August 21	August 28	Sept. 4	Sept. 11
Mr. McCoy						*	*			
Patricia McDaniel	Project Consultant				*	*				*
Dick Mertens	BRA				*					
Martin Murphy	Haley & Aldrich				*					
Anne Nemrow	Neighborhood Association of the Back Bay	*	*	*		*	*	*		
Dan Ocasio	Southwest Corridor/MBTA	*	*	*	*		*	*	*	*
Mr. Palmer							*			
Deborah Poodry	Project Consultant	*		*	*	*	*	*	*	*
Terry Rankine	Boston Society of Architects		*		*	*	*	*	*	*
Stu Robbins	Back Bay Federation	*		*	*	*		*	*	*
Mark Robitz	TAC			*	*	*		*		
Ed Saunders	Mass. Turnpike Authority	*	*		*	*	*	*	*	*
Libby Seifel	Tent City Task Force		*	*	*	*	*	*	*	*
Leif Skalleberg	Urban Engineering				*		*			

GOVDOC

BRA

1371